

**MURRINDINDI PLANNING SCHEME  
DEVELOPMENT PLAN  
LOW DENSITY RESIDENTIAL ZONE  
RACECOURSE ROAD AND LAWWRANCES ROAD  
TOWNSHIP OF YEA**

**APPROVED BY MURRINDINDI SHIRE COUNCIL  
1<sup>ST</sup> APRIL 2008**



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## CONTENTS

1	Introduction	3
2	Town context	3
	Plan 1 – Town context	4
3	Planning controls	5
4	Site description and analysis	
	ownership	5
	existing use	5
	Plan 2 – Ownership	6
	landform	7
	views to and from the subject land	7
	watercourses	7
	vegetation	7
	Plan 3 – Land Capability and Features	8
	access	9
	public open space	10
	water supply	10
	sewer and on site effluent disposal	10
	storm water	10
	amenity impacts	11
	built form and landscape character	11
5	The key elements of the plan	
	lot size	12
	road network	12
	Plan 4 – Proposed Road Network and Indicative Lot Layout	13
	pedestrian links	14
	public open space	14
	protection of designated waterways	14
	Plan 5 – Protection of Waterways and Vegetation	16
	protection of native vegetation and biodiversity	16
	water supply	16
	sewer	16
	drainage	16
	Plan 6 – Provision of Utility Services	17
	power and communications	18
	development guidelines	18
	staging of development	19
	Plan 7 – Possible Staging	20
6	The development plan	
	purpose	21
	objectives	21
	Plan 8 – The Development Plan	22
7	Plan review	
	conditions and requirements for permits	23
	what is the life of this plan	23

## **1. INTRODUCTION**

This Development Plan covers 89.6 hectares of land fronting the western side of Lawrances Road and the south side of Racecourse Road in the Township of Yea (the 'subject land').

The Development Plan has been prepared in consultation with the owners of the seven separate properties that together make up the subject land.

The Murrindindi Planning Scheme (the 'Planning Scheme') includes the subject land within a Low Density Residential zone and a Development Plan Overlay No 3. The zoning and overlay were applied to the land through Amendment C14 to the Murrindindi Planning Scheme, gazetted on 26<sup>th</sup> July 2006.

The Development Plan Overlay requires the preparation and approval of a development plan to guide and coordinate the subdivision and development of the whole of the land within the Overlay before approval can be given to develop any individual property.

This Development Plan satisfies the requirements of the Development Plan Overlay and facilitates the preparation and consideration of applications for planning permits for the coordinated, low-density residential subdivision and development of individual properties.

## **2 TOWN CONTEXT**

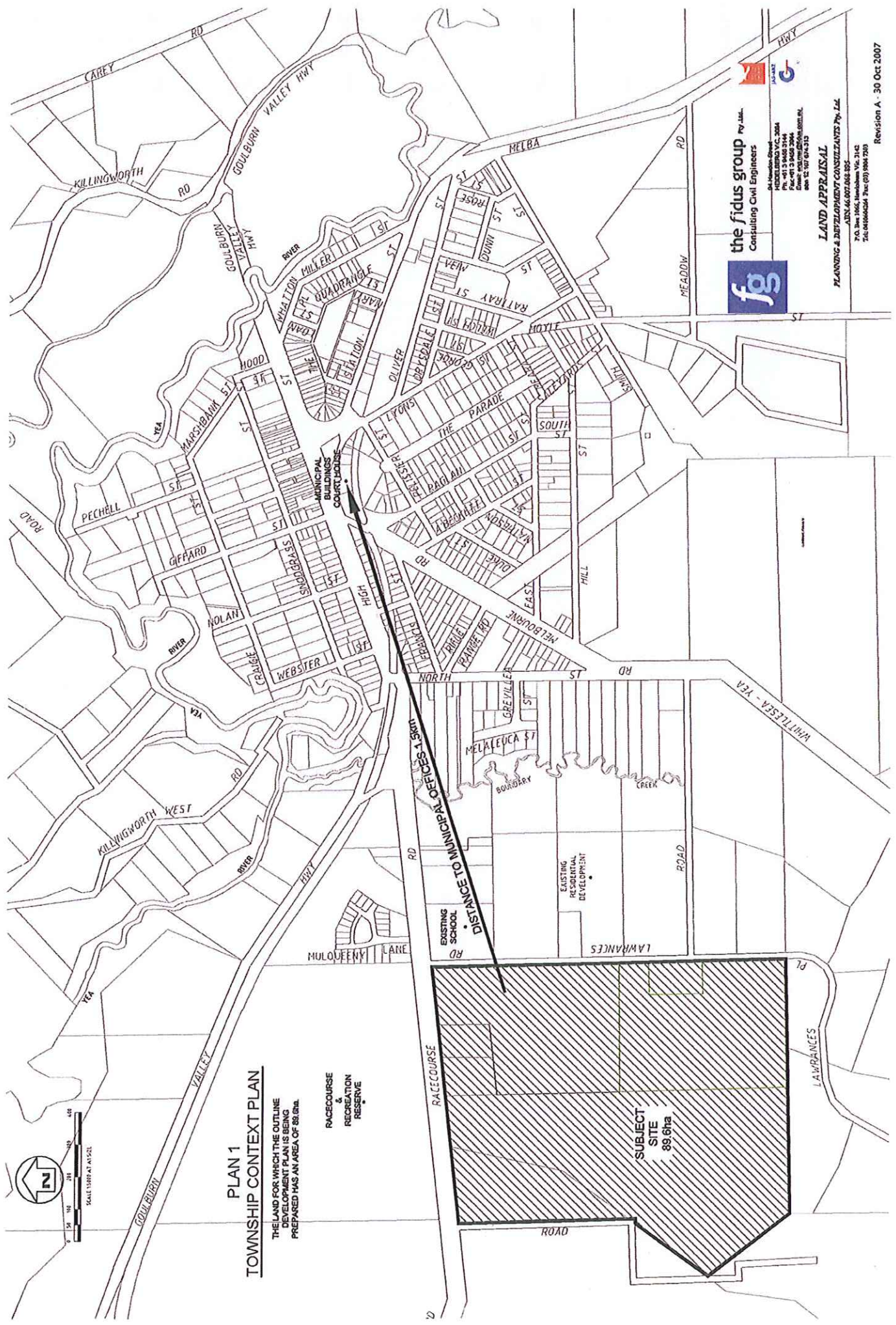
The Township of Yea is located on the Yea River at the intersection of the Goullburn Valley Highway and Melba Highway. The Murrindindi Planning Scheme identifies Yea as one of three growth areas in the Municipality. The subject land is located on the southwestern edge of the town approximately 1.5 kilometres from the Yea Post Office. The location of the subject land is shown on Plan 1.

The Yea Secondary College fronts the northeast corner of Racecourse Road and Lawrances Road. The land on the east side of Racecourse Road is included within a Residential 1 zone and is being developed for a range of conventional of residential lots that are generally 1,000sqm and smaller.

The Yea Racecourse and recreation reserve is located opposite the subject land to the north of Racecourse Road.

The subject land adjoins rural land on its south and west boundaries. The area supports broad acre grazing and rural residential properties rather than intensive agriculture. The properties to the south fronting Lawrances Road and west fronting Racecourse Road are included in a Rural Living zone. Within this zone land can be subdivided into lots with an average area of 6ha. The southwestern corner of the subject land adjoins land zoned Farming, a zone that has a 40ha minimum lot size.

Town services have been extended to the boundaries of the subject land. Water, sewer and drainage pipes are located at the intersection of Racecourse Road and Lawrances Road. Racecourse Road has a 60 metre reservation width and connects with the Goullburn Valley Highway, which is approximately 700 metres from the subject land.



**PLAN 1**  
**TOWNSHIP CONTEXT PLAN**

THE LAND FOR WHICH THE OUTLINE  
DEVELOPMENT PLAN IS BEING  
PREPARED HAS AN AREA OF 88.0ha.

RACECOURSE  
&  
RECREATION  
RESERVE

**SUBJECT  
SITE**  
88.6ha

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### **3 PLANNING CONTROLS**

The Murrindindi Planning Scheme (the 'Planning Scheme') includes the subject land within a Low Density Residential zone and a Development Plan Overlay No 3.

The Low Density Residential zone provides for low-density residential development on lot sizes that depend on the availability of services and capacity of the land and to accommodate development. The intention of the Planning Scheme is that the lots will be connected to reticulated services if they are available.

The Development Plan Overlay requires the preparation and approval of a development plan to guide and coordinate the subdivision and development of the whole of the land within the Overlay before approval can be given to develop any individual property.

Detailed design issues such as the layout of the lots and provision of services are considered at the planning permit stage. Within a Low Density Residential zone the subdivision of land into residential lots requires a planning permit. An application for a planning permit must be assessed against the requirements of Clause 32.03-3 and Clause 56.07-1 to 56.07-4.

Residential lots must be at least 0.4 ha. If reticulated sewer is not available all wastewater must be treated and retained on site in accordance with State Environment Protection Policy requirements. Without reticulated sewer lots will be generally up to 2ha in size but this is dependent on the results of a land capability assessment.

Native vegetation controls at Clause 52.17 of the Planning Scheme seek to:

- Avoid the removal of native vegetation;
- If the removal of native vegetation cannot be avoided, to minimise the removal of native vegetation through appropriate planning and design;
- Offset the loss of native vegetation by new planting.

### **4 SITE DESCRIPTION AND ANALYSIS**

#### **Ownership**

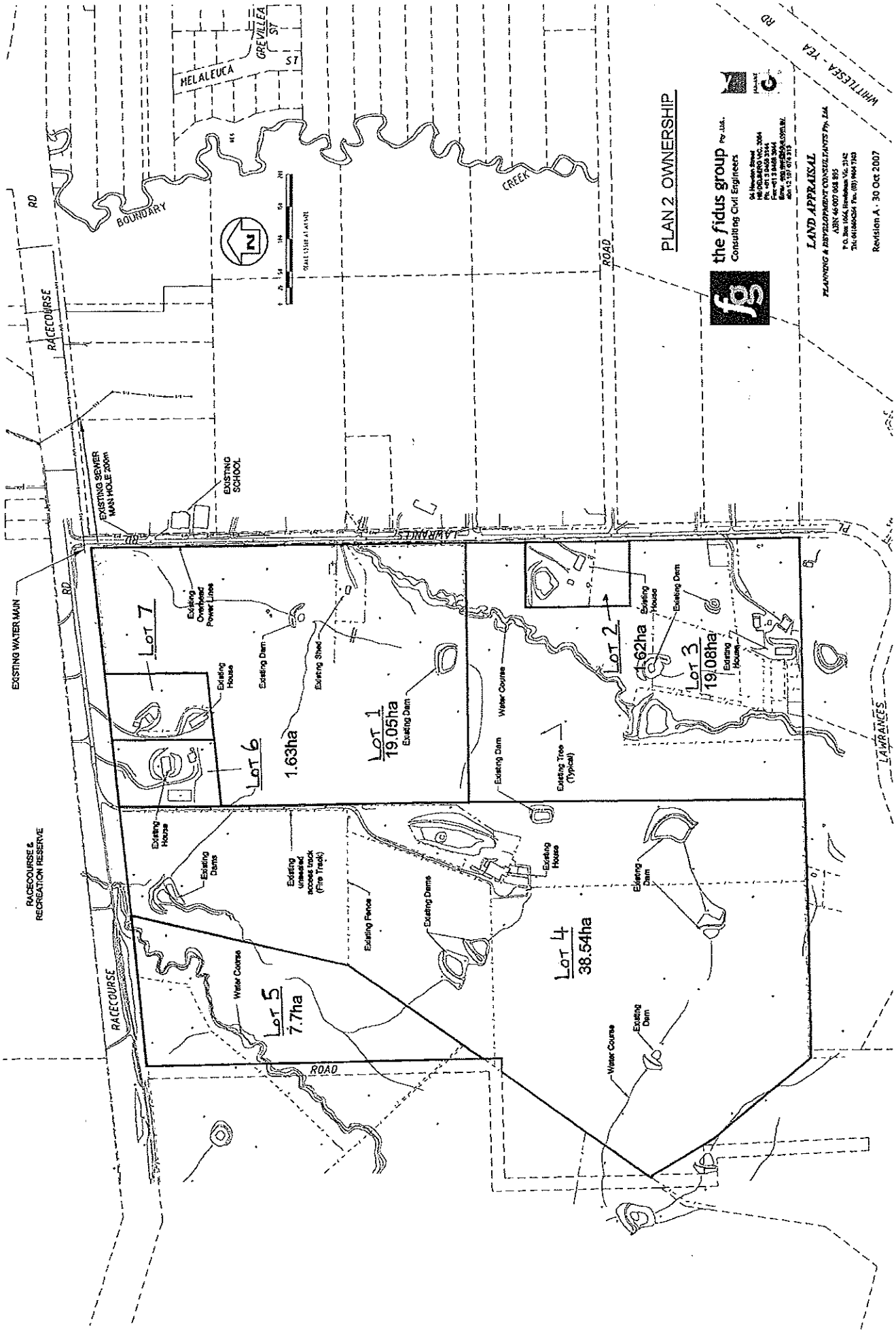
The subject land is divided into seven separate properties ranging from 1.6ha to 40ha. The existing pattern of ownership is shown on Plan 2.

#### **Existing Use**

The three smaller properties are used for rural residential purposes.

The property fronting the northeastern corner of Racecourse Road and Lawrances Road has been purchased for residential development. The property in the southeast corner of the subject land is used for horse breeding.

The property in the northwest corner of the subject land is part of a large commercially operated farm that extends into the adjoining Rural Living zone.



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The property that extends from Racecourse Road to the southwest corner is a large rural living lot, which is used for grazing.

Five of the seven properties contain existing houses together with associated farm buildings and dams.

### **Landform**

The subject land falls towards Racecourse Road is divided by a central north south ridgeline. The levels rise from a low point of 171m in the northeast corner to a high point of 262m in the southwest corner of the subject land. Refer to Plan No 3.

The slope of the subject land ranges from almost flat land with grades of 1 in 70 around the corner of Racecourse Road and Lawrances Road, through gentle to moderately sloping land on the sides and across saddles of the ridge line, to steeply sloping land with grades of up to 1 in 2 around the hill tops in the southwest corner.

### **Views to and from the subject land**

The undulating nature of the land provides the opportunity for new houses on low-density lots to enjoy attractive rural views with a northern aspect.

The natural topography avoids the potential for development of the subject land to have any impact on views from the Goulburn Valley Highway or the Yea town centre.

Restricting development on the hilltops in the southwestern corner of the subject land will ensure future houses sit below the skyline and not disrupt local views.

### **Watercourses**

The subject land contains two determined waterways and a number of smaller eroded channels and natural depressions. Goulburn Murray Water has confirmed the status of the determined waterways.

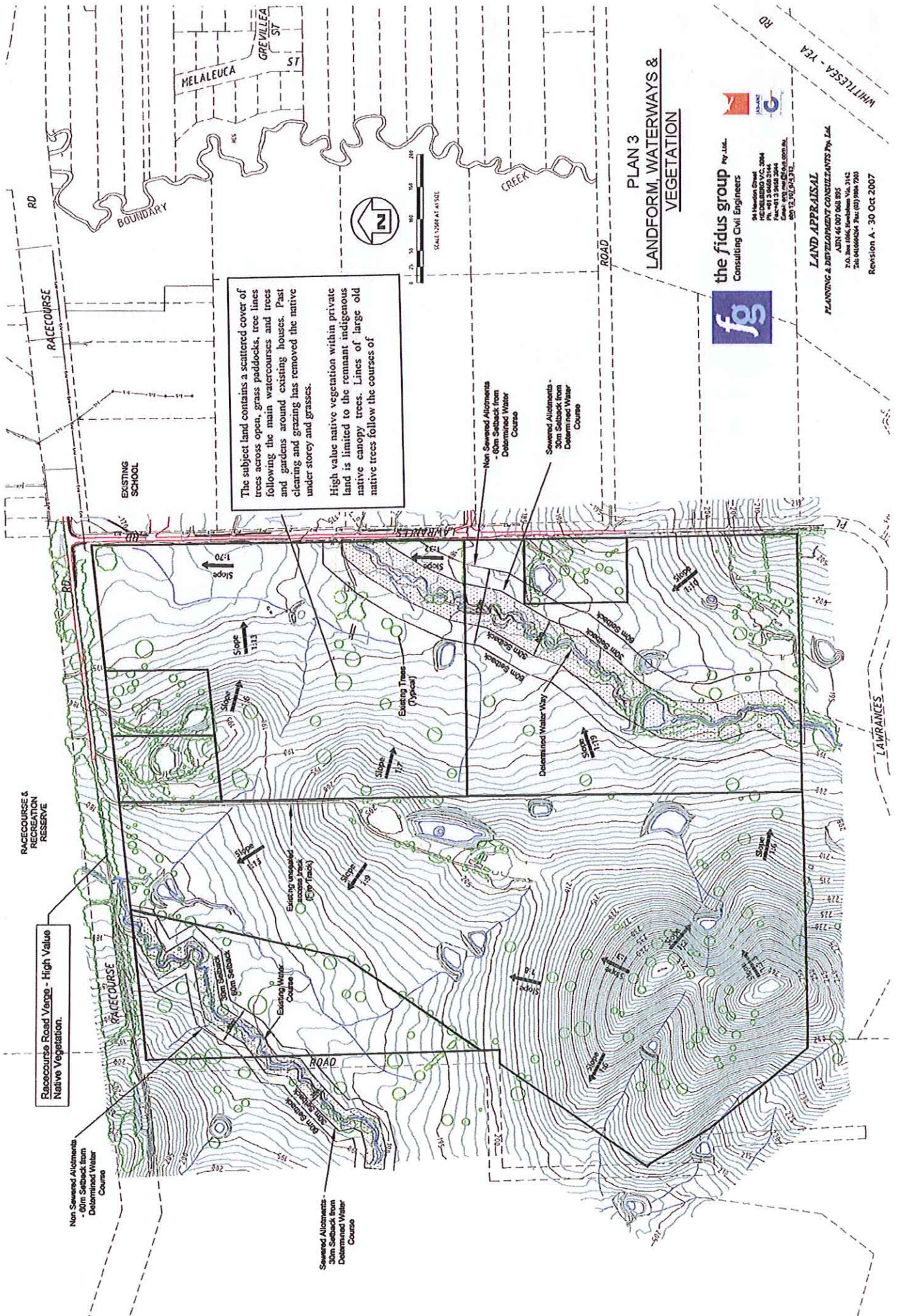
Determined waterways must be maintained and protected from development by buffer zones. The determined waterways have been eroded and degraded and are in need of remedial rectification works.

The natural flow of water across the subject land has been disrupted by the construction of existing farm dams.

### **Vegetation**

The subject land contains a scattered cover of trees across open, grass paddocks, tree lines following the main watercourses and trees and gardens around existing houses. Past clearing and grazing has removed the native under storey and grasses.

High value native vegetation within private land is limited to the remnant indigenous native canopy trees. Lines of large old native trees follow the courses of



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High value native vegetation within private land is limited to the remnant indigenous native canopy trees. Lines of large old native trees follow the courses of



SCALE 1:500 AT A1 SIZE

### PLAN 3 LANDFORM, WATERWAYS & VEGETATION

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the determined waterways.

High value native vegetation is present within the verge of Racecourse Road. The value of the vegetation stems from the survival of intact native, tree under storey and grass communities.

Non-indigenous native trees have been planted on a number of properties. While these trees add to the landscape they are not regarded as being high value vegetation.

Where scattered trees are to be lost, Department of Sustainability and Environment Net Gain Guidelines and the Port Phillip and Westernport Catchment Management Authority Draft Native Vegetation Management Plan state that the following offsets apply to medium conservation significance sites:

- For each large old tree cleared, 2 other large old trees must be protected;
- For each medium old tree cleared, 1 other medium old tree must be protected and 5 trees recruited; and
- For every one cm of dbh cleared, two trees/plants must be recruited / planted.

#### **Access**

Vehicle access is provided to the subject land from the east via Racecourse Road. This road has a 60m wide reservation containing a 6.2m wide bitumen pavement.

The bitumen pavement extends to the mid point of the Racecourse Road frontage of the subject land. Further to the west Racecourse Road narrows to a dirt track that extends through to the Goulburn Valley Highway.

Three properties have existing crossovers and driveways to Racecourse Road.

Lawrances Road has a 20m wide reservation containing a 5.2m wide bitumen pavement. Further south beyond the subject land Lawrances Road narrows to a gravel track and is terminated.

The existing road network including the intersection of Racecourse Road and Lawrances Road has the capacity to comfortably and safely accommodate increases in traffic generated by the development of the Subject Land. An assessment of the road network and impact of traffic generated by the development of the subject land has been prepared by Andrew O'Brien and Associates.

A network of shared pedestrian and bicycle trails is being constructed throughout the township of Yea. One of these trails extends from Boundary Creek to Lawrances Road opposite the subject land. Another trail extends from the Goulburn Valley Highway through The Habitat Estate to the north side of Racecourse Road.

On Racecourse Road the cleared area beneath the power lines provides an opportunity to extend a footpath past the site. Footpaths have not been constructed in Lawrances Road although adequate space exists within the reserve.

### **Public open space**

Yea Township is well provided with recreation reserves and linear park systems and trails. The subject land is located opposite the large Yea Racecourse reserve.

### **Water supply**

Existing water mains are located in Racecourse Road and also part of the way along Lawrances Road (refer to Plan 4). Goulburn Valley Water has determined that with current levels of development the existing water supply infrastructure can service up to the 185m contour. This level will decrease to the 175m contour, however once the residential land on the eastern side of Lawrances Road is developed.

New houses above the 175m contour will need to be served by the installation of a high-level storage tank. The hills in the southwest corner of the subject land provide the best location for a storage tank.

The Essential Services Commission has determined that while the high level water supply has the potential to serve a number of properties including the land on the east side of Lawrances Road is a local reticulation that must be paid for by the developers of the subject land.

### **Sewer and onsite effluent disposal**

An existing sewer main is located on Racecourse Road in front of the Secondary College. The sewer has sufficient capacity to accommodate the development on the subject land.

The east side of the ridgeline that divides the subject land falls towards the northeast allowing it to be connected to the sewer by a gravity main.

It is not anticipated that the land on the west side of the ridgeline will be connected to the sewer. Connecting the land on west side of the ridgeline to the sewer would require the installation of a rising main and pumping station.

Residential lots that are not connected to sewer must be sufficiently large to allow on site disposal of effluent in accordance with a land capability assessment that meets the requirements of State Environmental Policy.

### **Storm water**

The subject land is divided into three catchments.

The southeast catchment discharges into twin 1050mm diameter culverts under Lawrances Road. The twin 1050mm diameter culverts have sufficient capacity to convey the post development flows (100yr) to the east side of Lawrances Road.

The northeast catchment discharges into a single 450mm diameter culvert under Lawrances Road. The single culvert has insufficient capacity to convey the pre-development flows to the east side of Lawrances Road.

The northwest catchment discharges into an existing culvert under Racecourse Road.

### **Amenity impacts**

The low-density form of development minimizes the potential for detrimental amenity impacts between different lots within the subject land.

The gentle transition from the conventional residential lots east of Lawrances Road through the low-density lots on the subject land to the Rural Living Zone to the south and west and Farming Zone beyond limits the potential for conflicts between the subject land and surrounding area. The potential is further reduced by the fact that the surrounding rural area supports rural residential and livestock grazing rather than intensive agricultural uses.

### **Built form and landscape character**

The Township of Yea has an open rural character. Low-density residential development consists of predominantly single storey houses with spacious setbacks and non-intrusive fencing. New houses are typically contemporary designs with pitched, hip roofs and eaves using a mix of materials in generally muted tones.

The subject land presents an opportunity for low-density residential development that provides an attractive lifestyle choice for people who are seeking to live in a more spacious and attractive environmental setting.

The key landscape features of the area include the rural character, undulating landform, native vegetation, views and watercourses.

In sloping areas the best examples of new development is characterised by houses that are set into the natural contours avoiding excessive cut and fill batters and allow for the retention of existing vegetation.

## **5 THE KEY ELEMENTS OF THE PLAN**

### **Lot size**

The minimum lot size is 0.4ha in accordance with the provisions of the Low Density Residential zone. The size of the lots provided for by the Development Plan varies in response to the availability of reticulated sewer and the slope of the land. In particular:

- The land on the east side of the central ridgeline will accommodate lots with a minimum size of 0.4ha connected to reticulated sewer.
- The land on the west side of the ridgeline will accommodate larger lots in the order of 2ha with on site effluent disposal.
- Lot sizes will increase on steep slopes particularly on land with average grades in excess of 15% around the hilltops in the southwest corner.

An indicative lot layout for the land on the east side of the ridgeline is included in Plan 4. The layout is only indicative, as any future subdivision must be the subject of an application for a planning permit.

A lot layout has not been provided for the land to the west of the ridgeline. The size of these lots will be determined by a land capability assessment to demonstrate the capacity of the land to contain on site effluent disposal.

### **Road Network**

The internal road layout had been designed to ensure connectivity throughout the subject land while minimizing the need for earthworks and loss of native vegetation.

The east west road links follow the natural saddles across the ridgeline. One link road extends from Lawrances Road to the unmade government along the western boundary to facilitate the future development of the Rural Living zone further to the west.

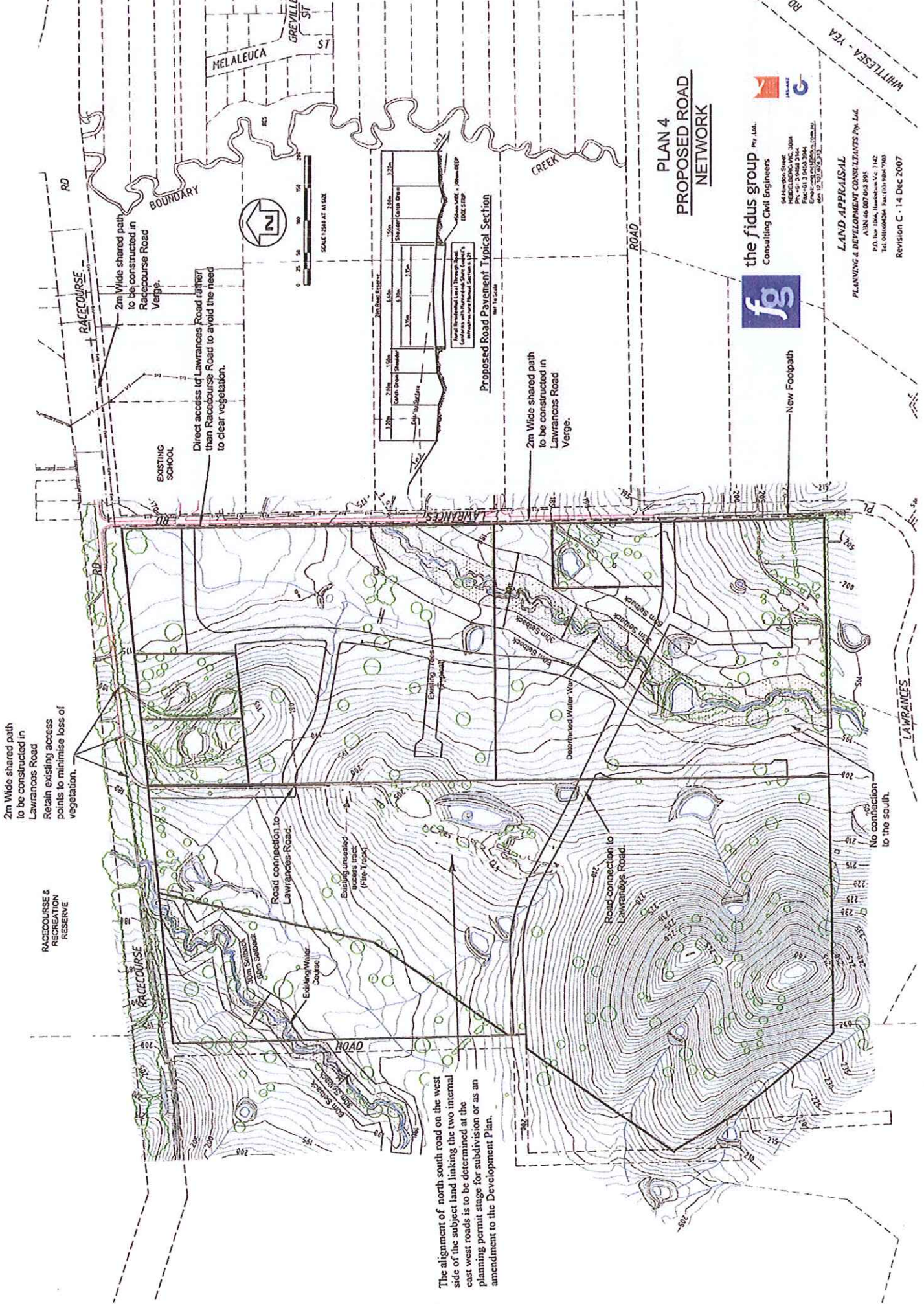
The north south link road follows an existing crossover to Racecourse Road and an access track on the west side of the ridgeline. The Development Plan requires the provision of a north south road on the west side of the subject land linking the two internal east west roads. The future alignment of the north south road is to be determined at the planning permit stage for subdivision or as an amendment to the Development Plan.

The road layout minimizes the loss of existing high value native vegetation by using the three existing crossovers on Racecourse Road rather than clearing new openings.

The detailed design of the roads must be in accordance with the Muririndindi Shire Council, Infrastructure Design Requirements for rural roads. All road reservations are to be 20m in width and pavement width to be 6.6m inclusive of concrete edge strip. New intersections are located at least 40m from exiting intersections to ensure safe sight lines and stopping distances.

2m Wide shared path to be constructed in Lawrance's Road. Retain existing access points to minimise loss of vegetation.

RACECOURSE & RECREATION RESERVE



The alignment of north south road on the west side of the subject land linking the two internal east west roads is to be determined at the planning permit stage for subdivision or as an amendment to the Development Plan.

### PLAN 4 PROPOSED ROAD NETWORK



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Revision C - 14 Dec 2007

### **Pedestrian links**

A shared footpath and bicycle path is provided in the verge of Lawrances Road. The shared path is to be 2m wide and constructed in crushed rock rather than a sealed bitumen or concrete surface.

A shared footpath and bicycle path is provided in the verge on Racecourse Road. The path will be located within the existing cleared area beneath the power lines to minimize the loss of vegetation.

### **Public open space**

No land has been set aside for public open space reserves within the subject land. As part of any future subdivision a monetary contribution will be made to the provision of recreation land and facilities in accordance with the provisions of the Subdivision of Land Act.

### **Protection of designated waterways**

The Development Plan identifies the two determined waterways within the subject land. The determined waterways are incorporated within the lots rather than a reserve. In accordance with the advice of Goulburn Murray Water (GMW) the waterways are protected by building exclusion zones that are:

- 30m if the lots are connected to reticulated sewer; and
- 60m wide if the lots are not connected to reticulated sewer.

Remedial works are required to rectify existing degradation within the waterways. The extent of these works will be determined by the Goulburn Broken Catchment Management Authority (GBCMA).

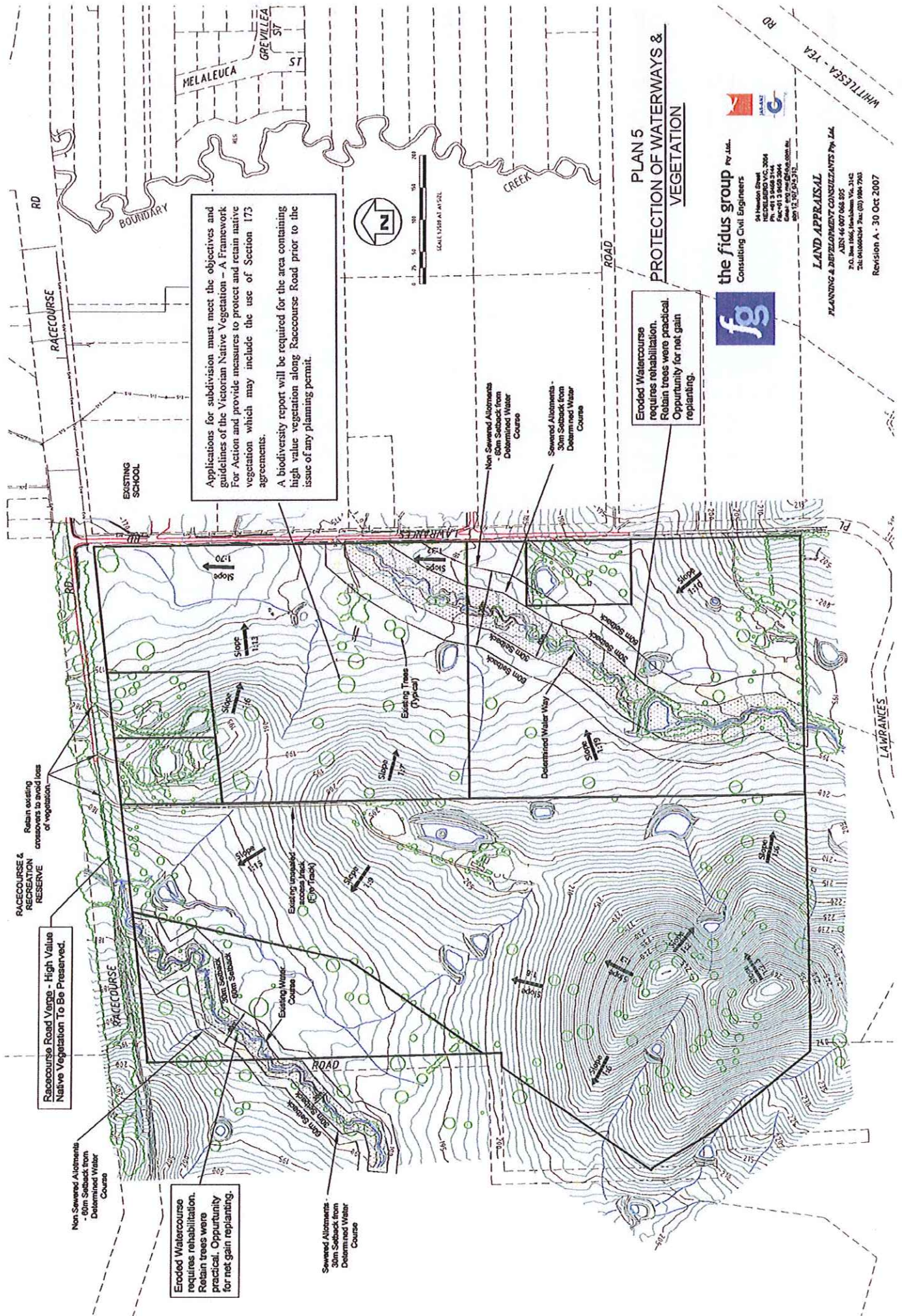
### **Protection of native vegetation and biodiversity**

The Development Plan has been designed to protect and retain native vegetation.

The high value native vegetation within the verge of Racecourse Road has been protected. In this respect:

- Connections between the internal roads and existing road network are provided via Lawrances Road rather than Racecourse Road where practical.
- The Development Plan makes use of the three existing crossovers to Racecourse Road rather than clear new openings.

Applications for subdivision must meet the objectives and guidelines of the Victorian Native Vegetation – A Framework For Action and provide measures to protect and retain native vegetation, which may include the use of Section 173 agreements. A biodiversity report will be required for the area containing high value vegetation along Racecourse Road prior to the issue of any planning permit.



Applications for subdivision must meet the objectives and guidelines of the Victorian Native Vegetation - A Framework For Action and provide measures to protect and retain native vegetation which may include the use of Section 173 agreements.

A biodiversity report will be required for the area containing high value vegetation along Racecourse Road prior to the issue of any planning permit.

**PLAN 5  
PROTECTION OF WATERWAYS &  
VEGETATION**

Eroded Watercourse requires rehabilitation. Retain trees where practical. Opportunity for net gain replanting.

Racecourse Road Verges - High Value Native Vegetation To Be Preserved.

Eroded Watercourse requires rehabilitation. Retain trees where practical. Opportunity for net gain replanting.

Sewered Alignments - 30m Setback from Determined Water Course

Non Sewered Alignments - 60m Setback from Determined Water Course

Sewered Alignments - 30m Setback from Determined Water Course

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The design and layout of subdivisions should seek to retain existing mature native canopy trees. Where the removal of native vegetation is unavoidable a net biodiversity gain shall be provided in accordance with Department of Sustainability and Environment Net Gain Guidelines. New developments will be required to include measures to protect existing trees that are intended for retention.

Recruitment and replanting of native vegetation should be carried out along the determined waterways on the subject land. The replanting should include under storey trees, different layers of shrubs and grasses as well as canopy trees to re-establish native vegetation communities on the subject land.

### **Water supply**

Each lot is to be provided with reticulated water. In order to service new houses on land above the 185m contour a new high level storage tank and pump is to be provided. The final location of the high-level water tank will be approved at the planning permit stage after full consideration of the options.

The high-level storage tank is to be included within a reserve in favour of Goulburn Murray Water and is to be provided with an all weather access track. The cost of the high level storage tank and pump is to be met by developers of the subject land.

### **Sewer**

Each lot that is between 0.4ha and 1ha is to be connected to reticulated sewerage. Larger lots shall be provided with an on site waste disposal system in accordance with a land capability assessment that meets the requirements of State Environmental Policy.

### **Drainage**

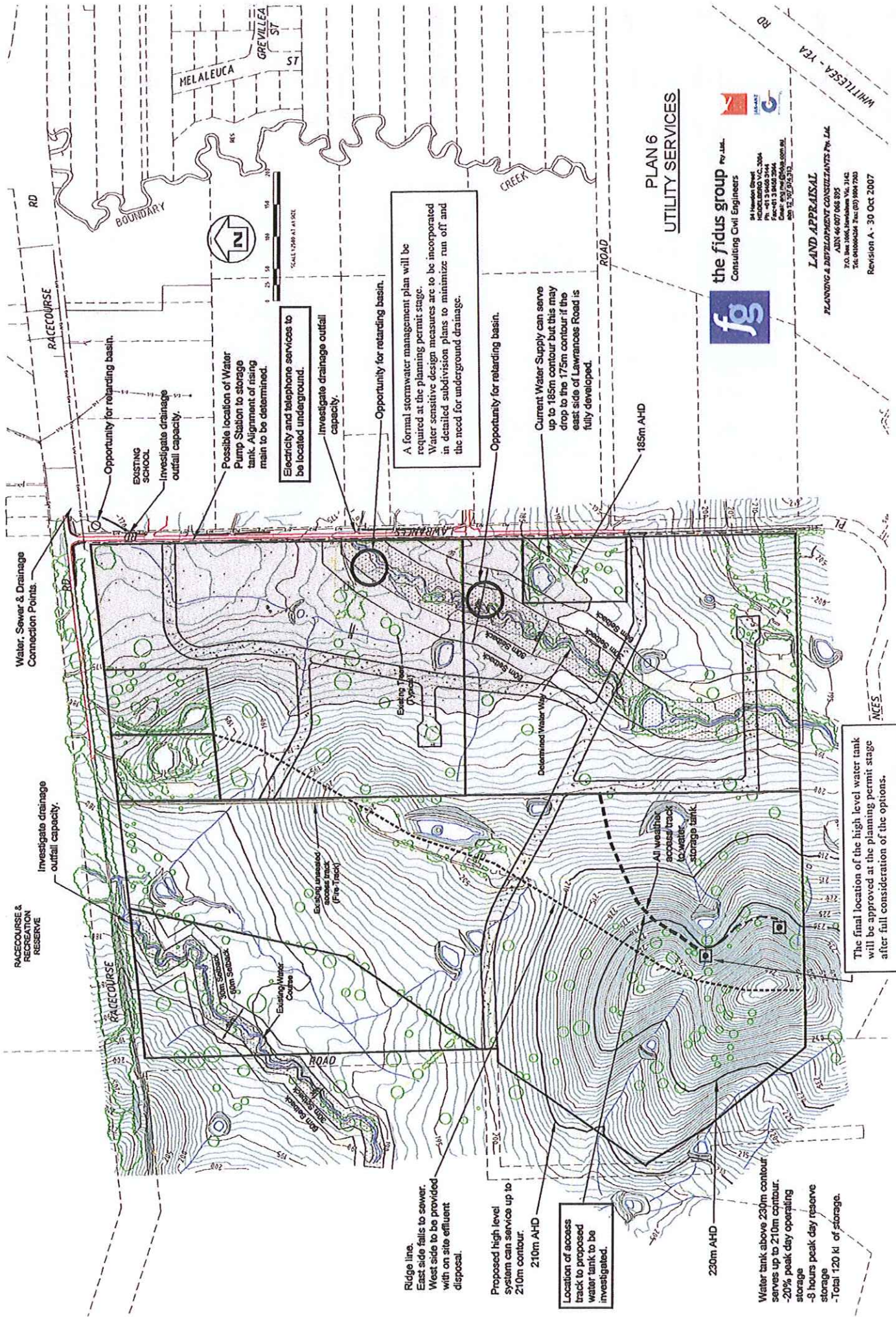
The proposed development will maintain existing / predevelopment run off volumes and or the capacity of downstream infrastructure. Water sensitive design measures are to be incorporated in detailed subdivision plans to minimize run off and the need for underground drainage.

Major flows will be conveyed via the road system discharging into the determined waterways. An open drain will be required to convey the major flow from road through a nominated easement within an allotment discharging into waterway.

A formal stormwater management plan will be required at the planning permit stage. The means of conveying minor flows will depend upon specific outfall conditions but incorporate the following:

- Flows from road pavement to swale drains and culverts in the road verge augmented by low flow underground pipes.
- Flows from lots grading to roads to swales and culverts in the road verge.
- Flows from lots grading to rear to underground pipes.
- Flows from lots adjacent to the determined waterways to bio-retention swales discharging directly into the waterway.





Water, Sewer & Drainage Connection Points

Investigate drainage outfall capacity.

RACECOURSE & RECREATION RESERVE

Opportunity for retarding basin.

EXISTING SCHOOL  
Investigate drainage outfall capacity.

Possible location of Water Pump Station to storage tank. Alignment of rising main to be determined.

Electricity and telephone services to be located underground.

Investigate drainage outfall capacity.

Opportunity for retarding basin.

A formal stormwater management plan will be required at the planning permit stage. Water sensitive design measures are to be incorporated in detailed subdivision plans to minimize run off and the need for underground drainage.

Opportunity for retarding basin.

Current Water Supply can serve up to 185m contour but this may drop to the 175m contour if the east side of Lawrance Road is fully developed.

185m AHD

Existing unsealed access track (Fire Track)

Existing Water Cough

Ridge line. East side falls to sewer. West side to be provided with on site effluent disposal.

Proposed high level system can service up to 210m contour. 210m AHD

Location of access track to proposed water tank to be investigated.

230m AHD

Water tank above 230m contour serves up to 210m contour. -20% peak day operating storage -8 hours peak day reserve storage -Total 120 kl of storage.

The final location of the high level water tank will be approved at the planning permit stage after full consideration of the options.

PLAN 6  
UTILITY SERVICES

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Further investigation is required at the detailed subdivision design stage to determine the down stream capacity of the drainage system on the east side of Lawrances Road and the capacity of the culvert and drainage system on the north side of Racecourse Road.

### **Power and communications**

Each lot will be connected to an underground electricity supply and telephone service.

### **Development guidelines**

The subject land presents the opportunity for low-density residential development that provides an attractive lifestyle choice for people who are seeking to live in spacious and attractive environmental setting.

The key landscape features of the area include the rural character, undulating landform, native vegetation, views and watercourses.

The desired outcome is housing that sits comfortably in the landscape through generous setbacks, the sensitive siting of buildings, access and earthworks, unobtrusive fencing and the restoration of native vegetation. Before deciding an application for subdivision the Responsible Authority must consider as appropriate:

- The provision of building envelopes that cover a maximum of 25% of the lot.
- The provision of driveway envelopes that cover a maximum of 7% of the lot.
- Whether the subdivision will result in tree removal that will be detrimental to the landscape character of the area;
- Locating buildings and works on land with slopes less than 20%;
- The desirability of ensuring buildings and works on land with an average slope greater than 10% are sited parallel with the contours;
- The need for building colours and materials to be non reflective; and
- The need to include controls by way of a Section 173 Agreement to require a planning permit to;
  - Construct a building with a height of more than 8m above the natural surface of the ground directly below that part;
  - Construct a building within 10m of any boundary abutting a road;
  - Construct a building within 5m of a side or rear boundary not abutting a road;
  - Construct a retaining wall or cut and fill batter with a height exceeding 1.5m;
  - Construct a shed or outbuilding in excess of 40sqm unless attached by walls, roof and floor to a dwelling and is constructed in materials which match the dwelling;
  - Construct a fence other than a farm fence that does not exceed a height of 1.2m and is at least 60% transparent.

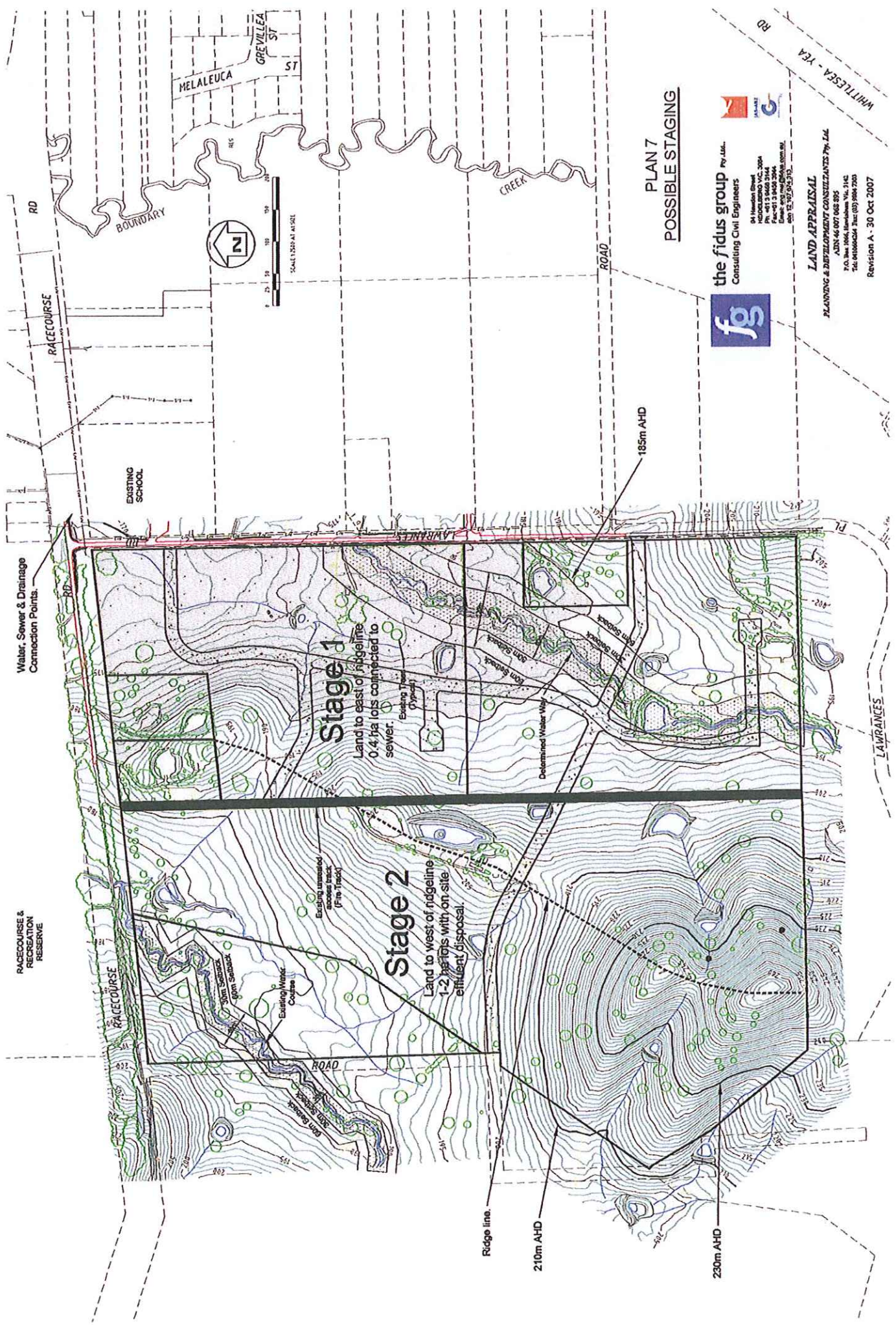
### **Staging of development**

The subject land is physically divided into two by the north south ridgeline. Existing utility services must be extended to the subject land from the northeast corner.

Given the natural landform and location of existing services it is reasonable to expect that development of the subject land may proceed in two main stages with development starting east of the ridgeline and building on existing service connections.

Plan 7 provides for the possible staging of development with Stage 1 being land to the east of the ridgeline and Stage 2 being the land to the west of the ridgeline.

The staging is only indicative. Stage 1 does not have to proceed before Stage 2. Development may proceed in different or smaller stages depending on the timing and availability of services and proposals from different landowners.



Water, Sewer & Drainage Connection Points.

RACECOURSE & RECREATION RESERVE

EXISTING SCHOOL

**Stage 1**  
Land to east of ridge line  
0.4 ha lots connected to sewer.

**Stage 2**  
Land to west of ridge line  
1-2 ha lots with on site effluent disposal.

**PLAN 7**  
**POSSIBLE STAGING**

**the fidus group** Pty. Ltd.  
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Revision A - 30 Oct 2007



## 6 THE DEVELOPMENT PLAN

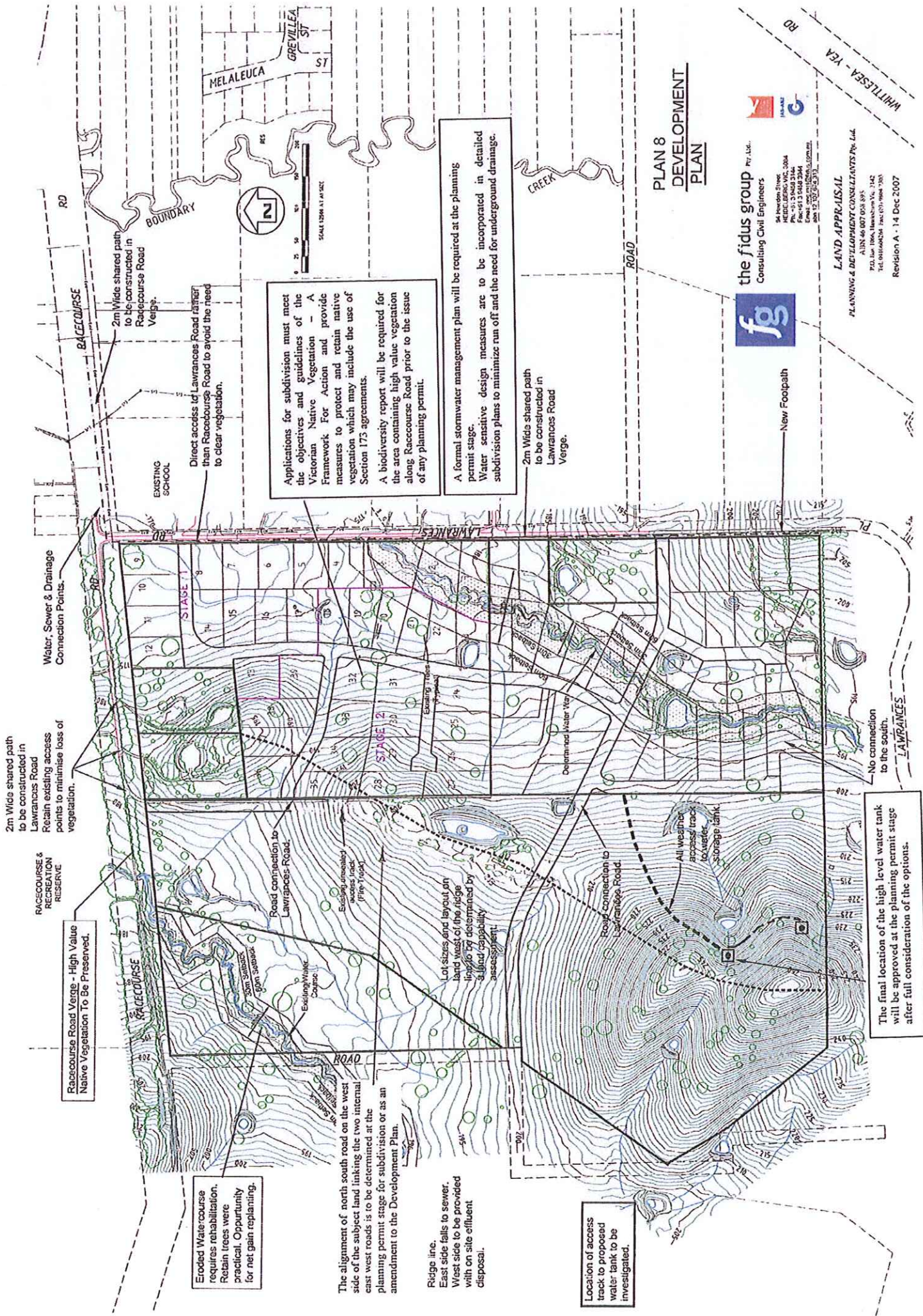
### Purpose

The purpose of the Racecourse Road Lawrances Road Development Plan is to provide for low-density residential development in a coordinated, efficient and site responsive manner in accordance with the provisions of the Murrindindi Planning Scheme.

### Objectives

The objectives of the development plan are to:

- Provide for low-density residential development on lots, which in the absence of reticulated sewerage, can treat and retain all wastewater.
- Provide a range of lot sizes reflecting variations in the natural capacity of the land to and services accommodate development.
- Maximise the efficient provision of utility services including:
  - reticulated water to each lot;
  - reticulated sewer to lots between 0.4ha and 1 ha;
  - water sensitive design to reduce storm water run off;
  - underground power and communications services.
- Provide for residential development without reticulated sewer on larger lots capable of treating and retaining wastewater on site in accordance with the requirements of State Environmental Policy.
- Provide safe and efficient vehicle access with sealed north south and east west internal roads linking to the existing road network via intersections with good sight lines and safe separation distances.
- Provide good pedestrian and bicycle access by creating links to the existing and planned shared pedestrian and bicycle trail network for the Yea Township.
- Retain and enhance the biodiversity and native vegetation values of the land by ensuring applications for subdivision meet the objectives and guidelines of the Victorian Native Vegetation – A Framework For Action, provide measures to protect and retain native vegetation including the use of Section 173 agreements and, where relevant, provide a biodiversity report for the area containing high value vegetation along Racecourse Road.
- Protect and enhance designated waterways through the provision of building buffer zones, remedial works and replanting of native vegetation.
- Contribute to the spacious, rural township built form character of low-density residential areas in Yea.
- Facilitate the efficient and timely provision of utility services through the staging of development east and west of the central ridgeline.



2m Wide shared path to be constructed in Lawrance Road. Retain existing access points to minimise loss of vegetation.

RACECOURSE & RECREATION RESERVE

Racecourse Road Verge - High Value Native Vegetation To Be Preserved.

Eroded Water-course requires rehabilitation. Retain trees where practical. Opportunity for net gain replanting.

The alignment of north south road on the west side of the subject land linking the two internal east west roads is to be determined at the planning permit stage for subdivision or as an amendment to the Development Plan.

Ridge line. East side falls to sewer. West side to be provided with on site effluent disposal.

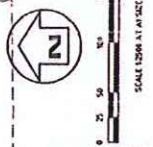
Location of access track to proposed water tank to be investigated.

Applications for subdivision must meet the objectives and guidelines of the Victorian Native Vegetation - A Framework For Action and provide measures to protect and retain native vegetation which may include the use of Section 173 agreements. A biodiversity report will be required for the area containing high value vegetation along Racecourse Road prior to the issue of any planning permit.

A formal stormwater management plan will be required at the planning permit stage. Water sensitive design measures are to be incorporated in detailed subdivision plans to minimize run off and the need for underground drainage.

2m Wide shared path to be constructed in Lawrance Road Verge.

The final location of the high level water tank will be approved at the planning permit stage after full consideration of the options.



**PLAN 8 DEVELOPMENT PLAN**

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 Revision A - 14 Dec 2007

## 7 PLAN REVIEW

### Conditions and requirements for permits

Applications for planning permits for any use or development of land covered by this development plan must be generally in accordance with this development plan. Only proposals that generally conform to the development plan may be approved in accordance with it.

When considering an application for planning permit is 'generally' In accordance with the approved development plan, the responsible authority must consider:

- Whether the proposal retains the ability to fully subdivide and develop either now or in the future in accordance with the applicable zone;
- Meets the objectives, conditions and general layout of the approved development plan.

The responsible authority should refuse any proposal that is not generally in accordance with the approved development plan.

If a proposal is not in accordance with an approved development plan, the responsible authority must amend the development plan before the proposal is considered and approved.

### What is the life of this plan?

This Development Plan will expire after twenty (20) years from the date of the approval of the Development Plan by Murrindindi Shire Council.

After the expiry of this development plan, the responsible authority may review and approve a new development.

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**DEVELOPMENT PLAN APPROVED BY MURRINDINDI SHIRE COUNCIL  
ON:**

..... 1st April 2008 .....

**SIGNED:**.....  .....

(CHIEF EXECUTIVE OFFICER)

**DATE:** 21-7-08